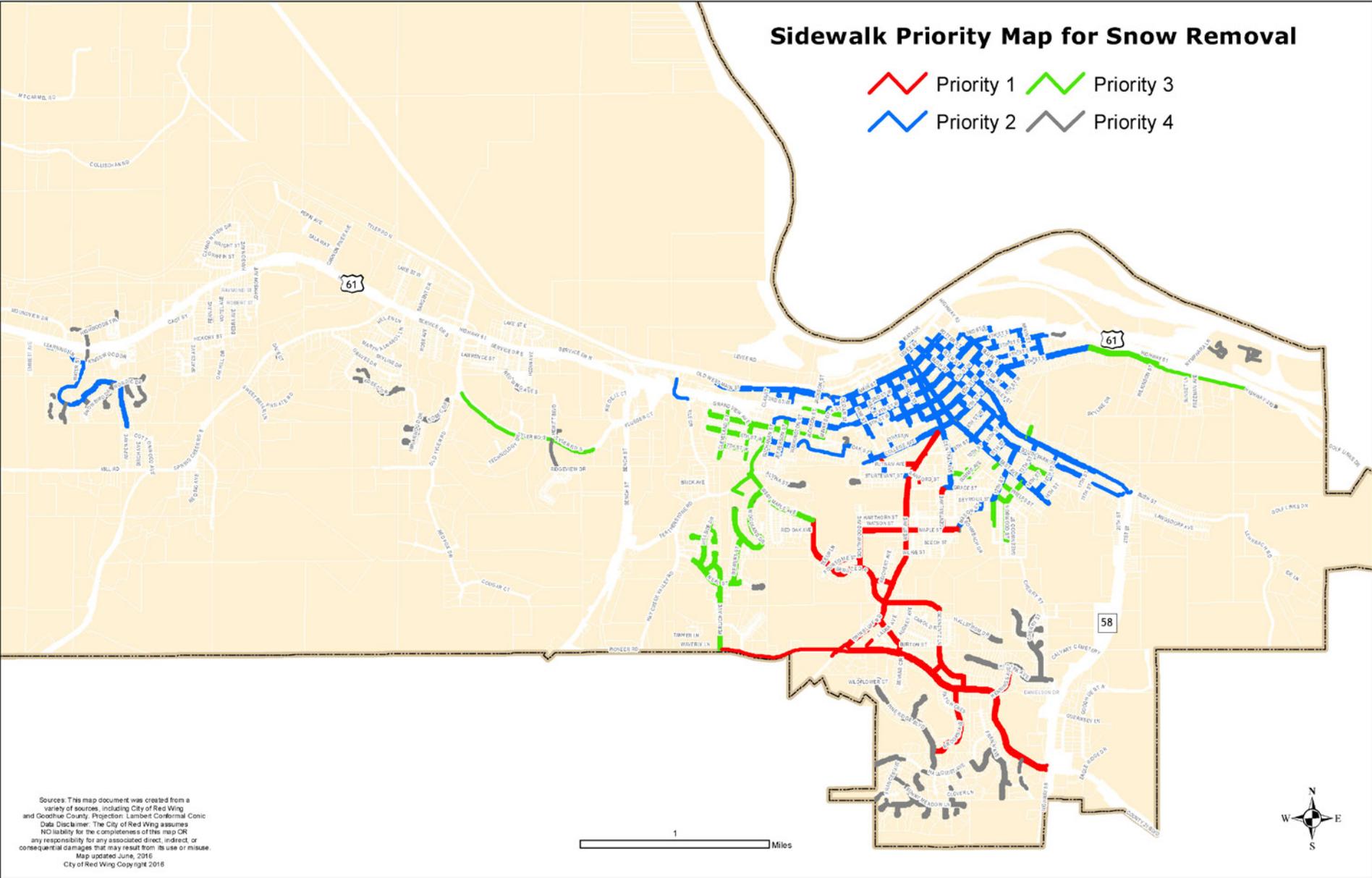






**APPENDIX A**  
**WINTER SIDEWALK MAINTENANCE**

# APPENDIX A





DATE \_\_\_\_\_

Dear \_\_\_\_\_:

Because you are a landlord with property within the City of Red Wing, we are sending you this reminder about the importance of **shoveling snow and ice from the sidewalks on all your properties in a timely manner.**

We're all responsible for keeping sidewalks safe. It's for your own benefit and for everyone who depends on sidewalks, including children, the elderly, and those who use wheelchairs and strollers. **If you have kept your walks clear of snow and ice in the past, we thank you and appreciate your efforts.**

Red Wing's policy states:

"Property owners must shovel their sidewalk **within 24 hours of a snowfall.** City crews may locate and shovel private sidewalks that have not been cleared so that sidewalks remain safe and accessible for everyone, including children."

**Any amount of snow or ice constitutes a snowfall, and snow must be removed from the entire width of the sidewalk. If snow is not shoveled from a property, and our City crews remove the snow, you will be invoiced \$120.** That invoice will go to you because you are the property owner on file, not your tenant(s). Crews photograph or videotape each snow removal and that record is always available for your review by contacting the Public Works Office.

The City does provide **free sand** for your sidewalks that can be picked up at the Public Works Office at 229 Tyler Road North, or the City Maintenance Building at 321 Bluff Street. The City is also compiling a **Shoveling Resource List** for property owners who need help shoveling their walks. For that information, please call the Public Works Department at 651-385-3674.

For the safety of your tenants and our City residents, we appreciate your time and attention to this matter. If you have questions, please call me at 651-385-3674.

Sincerely,

Lynn Nardinger  
Deputy Director of Public Services  
Public Works Department, City of Red Wing



## REMINDER TO PROPERTY OWNERS: THANKS FOR SHOVELING YOUR SIDEWALK

As a community we value winter walking, and we're all responsible for keeping sidewalks safe. It's for your benefit and for everyone who depends on sidewalks, including children, the elderly, and those who use wheelchairs and strollers. **If you have kept your walks clear of snow and ice in the past, we thank you and appreciate your efforts.**

Red Wing's policy states:

**"Property owners must shovel their sidewalk within 24 hours of a snowfall.** City crews may locate and shovel private sidewalks that have not been cleared so that sidewalks remain safe and accessible for everyone, including children."

**Any amount of snow or ice constitutes a snowfall, and snow must be removed from the entire width of the sidewalk.** If snow is not shoveled from a property and our City crews remove the snow, you as the property owner on file will be **invoiced \$120.** Crews photograph or videotape each snow removal and that record is always available for your review by contacting the Public Works Office.

The City does provide **free sand** for your sidewalk that can be picked up at the Public Works Office at 229 Tyler Road North, or the City Maintenance Building at 321 Bluff Street. The City is also compiling a **Shoveling Resource List** for people who need help shoveling their walks. For that information, please call the Public Works Department at 651-385-3674.

For the safety of everyone, we appreciate your time and attention to this matter. If you have questions, please call me at 651-385-3674.

Sincerely,

Lynn Nardinger  
Deputy Director of Public Services  
Public Works Department, City of Red Wing

December 1, 2015

# Remember to Shovel Your Sidewalk!

*It's the neighborly  
thing to do.*

Many people, including children,  
the elderly, and those using  
wheelchairs and strollers depend  
on sidewalks all year long.

**So be Red Wing nice.  
Clear your snow and ice!**



*(And if you have a neighbor who needs help shoveling, consider lending a hand.)*

To keep Red Wing's sidewalks safe and pleasant for everyone,  
Red Wing's policy states that all property owners must clear their  
sidewalk within **24 hours of snow or ice**. City crews will inspect and  
shovel private sidewalks that have not been cleared, and  
property owners will receive an invoice for \$120.

**Would you consider shoveling sidewalks for those who can't?  
It's a great way to volunteer or earn money  
for you or your organization.  
Email: [walkbikeredwing@gmail.com](mailto:walkbikeredwing@gmail.com)  
to be added to the Shoveling Resource List.**



The City of Red Wing provides **free sand** at  
229 Tyler Road North or 321 Bluff Street.

*Policy affects all property owners,  
including landlords.*

**Questions? Call Public Works at 651-385-3674.**





**APPENDIX**

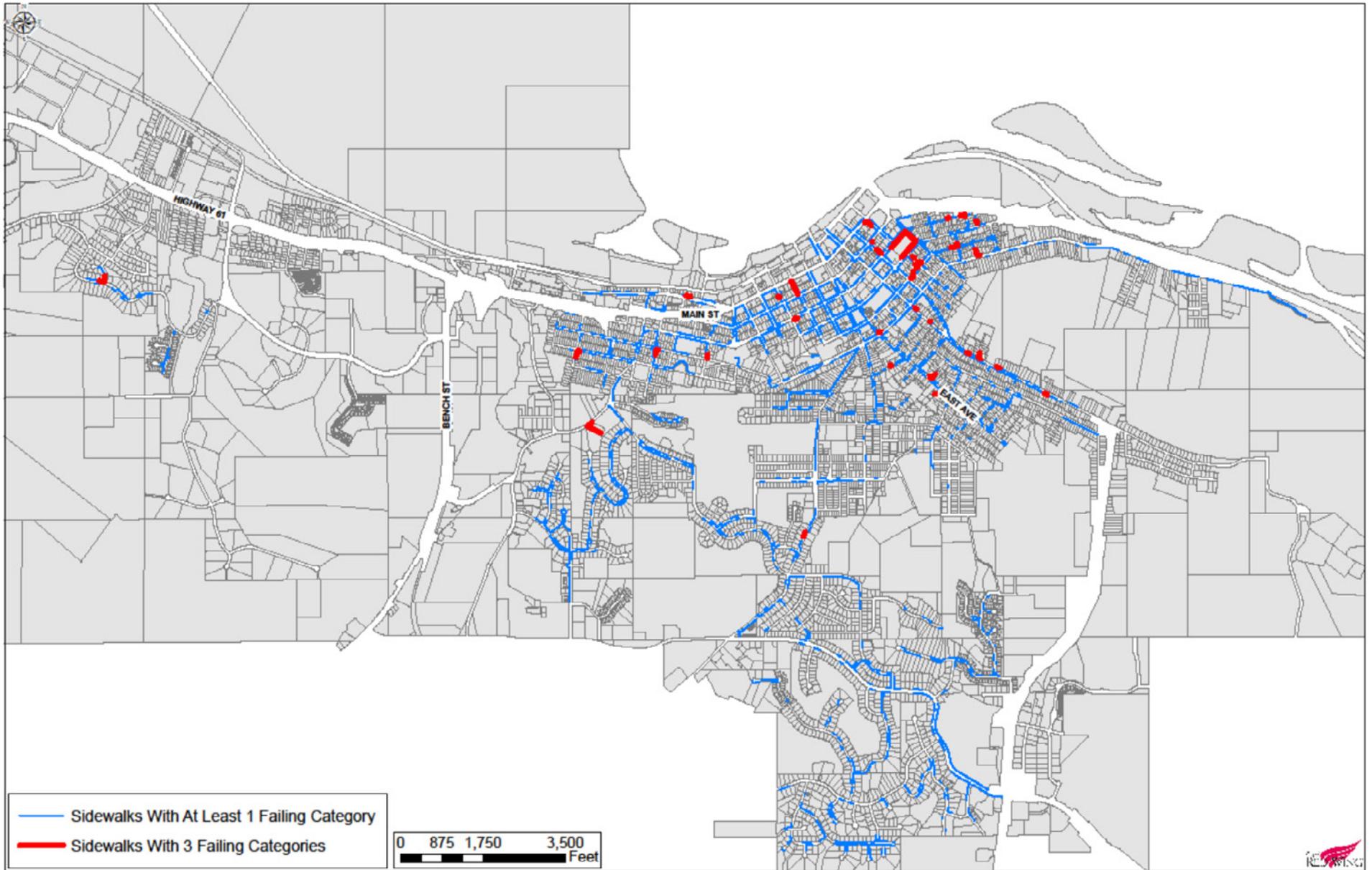
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**SIDEWALK INSPECTION & MAINTENANCE**

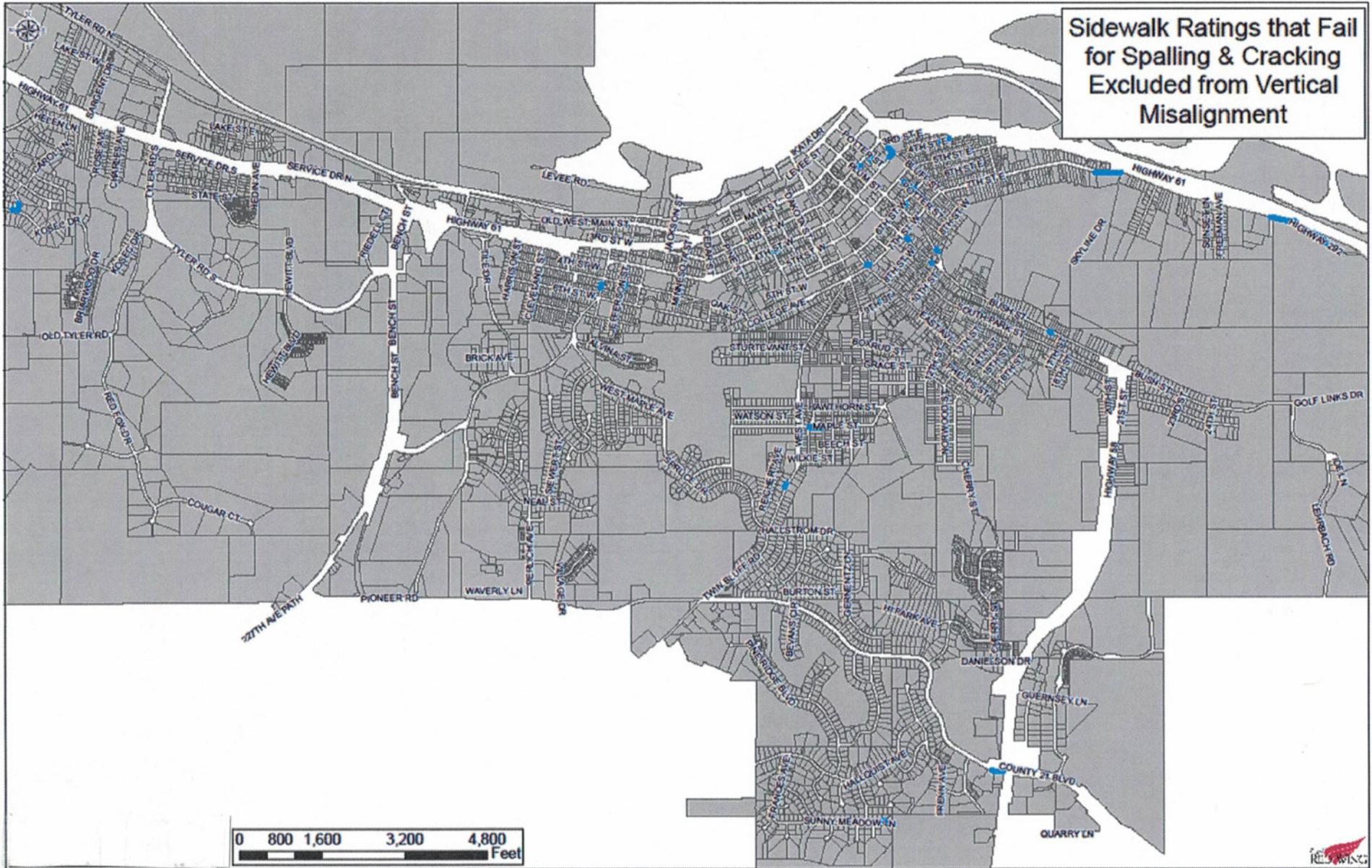
# APPENDIX B

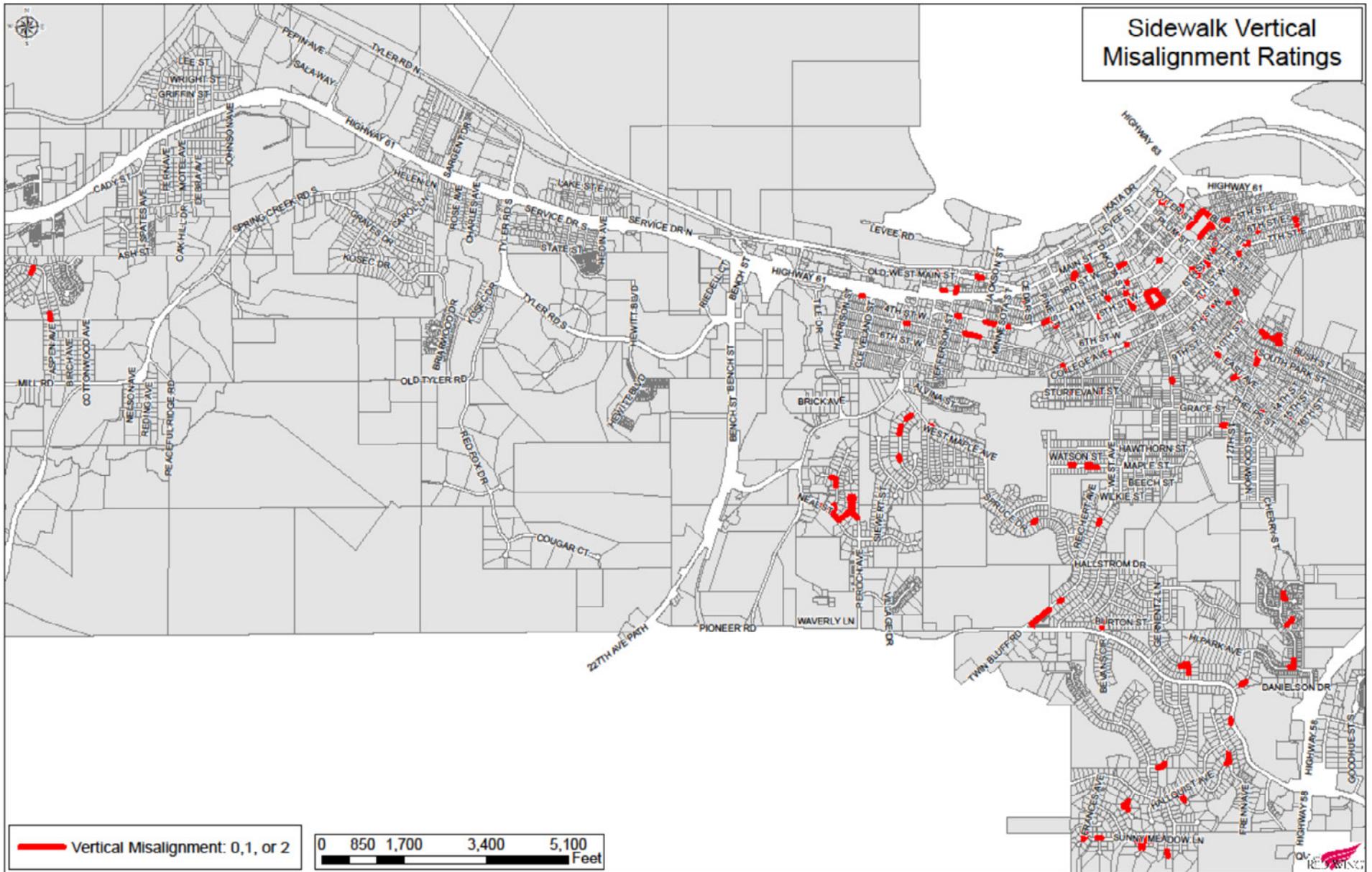
April – May	June	July	November	January	April – Aug.	Aug. – Oct.
<p>Public Works inspects one-third of city’s sidewalks; inputs data into electronic sidewalk database (Cartegraph)</p>	<p>Public Works Asset Specialist compiles report of sidewalks incorporating new spring data and provides report and analysis to staff Complete Streets Committee</p> <p><i>(Committee consists of a representative from Planning, Public Works, Police, and Engineering)</i></p> <p>Staff committee decides on resident-funded sidewalk projects for upcoming year</p>	<p>7/15: Public Works sends notification letters to property owners who need to repair their sidewalk</p> <p>(Number of owners varies; there will be more in first five years)</p>	<p>11/1: Deadline for residents to respond to City on how they choose to do repairs</p> <p>11/15: Public Works sends reminder letter to residents who haven’t responded</p>	<p>Complete Streets Committee outlines scope of the year’s resident-funded sidewalk project based on responses from residents and how many sidewalks are involved</p>	<p>City-hired contractor and/or Public Works staff implement repairs</p> <p>Public Works bills residents after work is completed</p> <p>Starting 7/15: Engineering Dept. meets with property owners who are using individual contractors to ensure projects are installed correctly and that projects are completed</p> <p>-----</p> <p>Public Works inspects another one-third of city’s sidewalks to keep database information up-to-date</p>	<p>Starting 8/1: Public Works inspects sidewalks that weren’t completed by residents in the necessary timeframe and makes repairs</p> <p>Public Works sends bills to those property owners</p>

**Recommended Ongoing Procedure for Resident-Funded Sidewalk Projects**

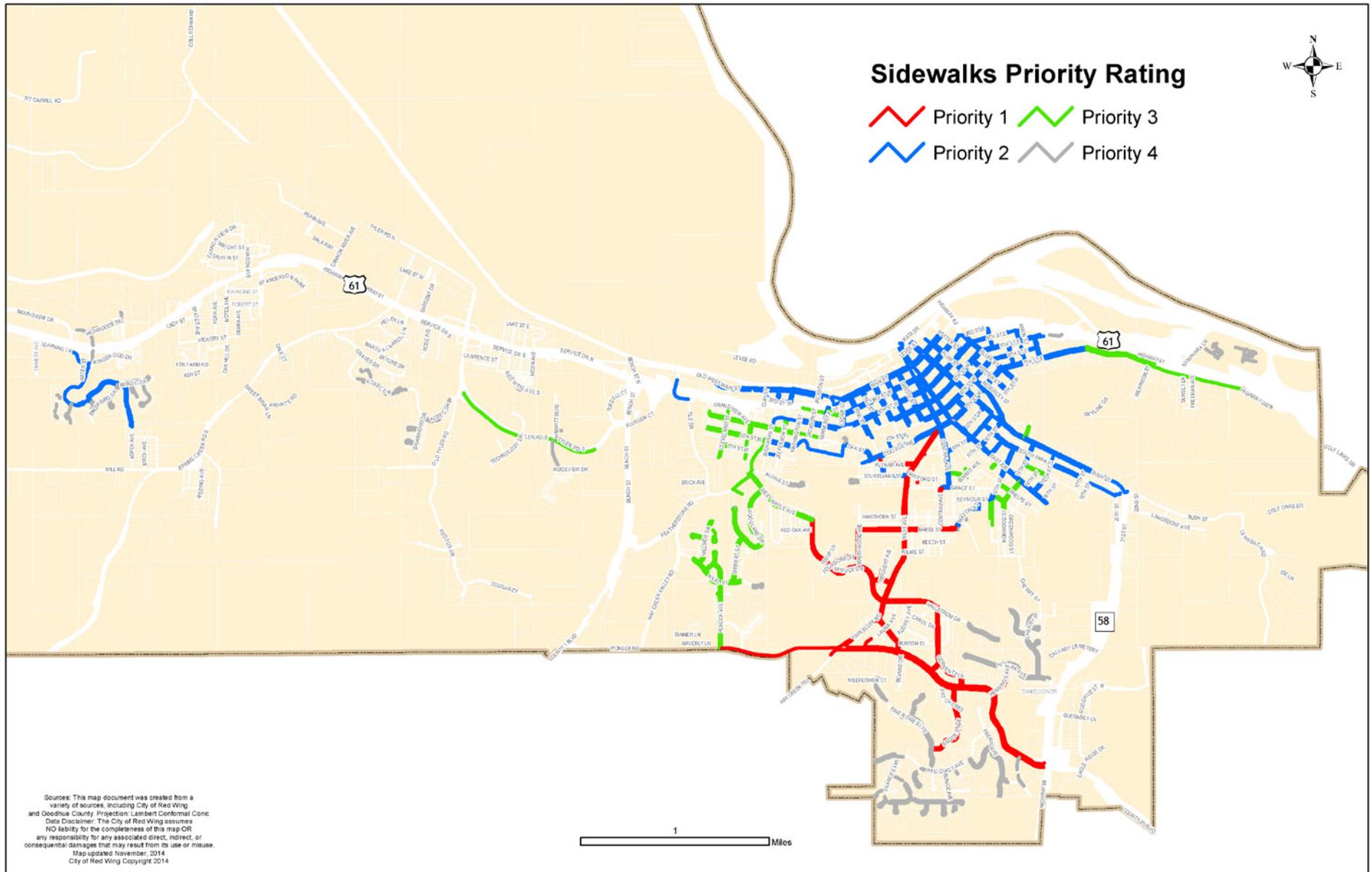


# APPENDIX B





# APPENDIX B



**Sidewalk Inspection Standards**

**Cracking**

Fail (no stars)	Cracked into 4 or more sections and/or gaps >2" wide
1 star	Cracked into 4 or more sections or gaps >1" wide
2 stars	Cracked into 3 sections with gaps <1" wide
3 stars	Cracked into 2 sections with gaps <1" wide
4 stars	Cracked into 2 sections with no gaps
5 stars	No cracks

**Gaps**

Fail (no stars)	Gaps >2" in width
1 star	2"
2 stars	1.5"
3 stars	1"
4 stars	.5"
5 stars	No gaps or <.5"

**Spalling**

Fail (no stars)	Sidewalk is impassable to wheeled travel
1 star	Surface is granular or multiple chunks of surface > 2" has broken out
2 stars	Surface is pitted. Chunks that have broken out are < 1" in size.
3 stars	Over 1/2 of sidewalk surface shows pitting, but no chunks of concrete have broken out.
4 stars	Sidewalk surface shows minimal pitting and no chunks of concrete have broken out.
5 stars	No spalling, surface of concrete is smooth

**Ramp Condition**

Fail (no stars)	Ramp is impassable for most wheeled travel
1 star	Ramp has multiple cracks creating rough terrain, concrete chunks missing or surface is spalling, obstacles create difficult navigation, curb lip is more than 1"
2 stars	Ramp has multiple cracks creating rough terrain, concrete chunks missing or surface is spalling, obstacles create difficult navigation, curb lip is more than 1/2"
3 stars	Cracks create unlevel ramp surface, weeds may be present in cracks
4 stars	No cracks, no obstacles, lip at curb line between 1/4" and 1/2"
5 stars	No cracks, no obstacles, less than 1/4" lip at curb line

**Traverse Slope**

Fail (no stars)	> 5/8" per foot
1 star	
2 stars	
3 stars	< 5/8" per foot
4 stars	
5 stars	< 3/8" per foot

**Vertical Misalignment**

Fail (no stars)	>2"
1 star	2"
2 stars	1.5"
3 stars	1"
4 stars	.5"
5 stars	No misalignment or <.5"

**Truncated Dome**

Fail (no stars)	Truncated domes non-functional
1 star	Truncated domes are spalling
2 stars	Truncated domes show cracks with gaps >1"
3 stars	Truncated domes show some cracks with gaps no more than .5"
4 stars	Truncated domes show some cracks with no gaps
5 stars	Truncated domes performing as designed

**Vegetation Overgrowth**

Fail (no stars)	Sidewalk is impassable
1 star	Grass has impeded onto sidewalk > 6" and trees/shrubs block pedestrian access
2 stars	Trees/shrubs block pedestrian access
3 stars	Grass has impeded onto sidewalk > 6"
4 stars	Grass has impeded onto sidewalk < 6"
5 stars	Sidewalk shows no narrowing due to overgrowth

# APPENDIX B



Date

Address  
Address  
Address

Property Location: Address Here

Dear Property Owner:

The City of Red Wing is continuing work to make our community a more walkable place for all residents. In 2015, every sidewalk within the city limits was inspected, entered into a database, and scored in multiple categories to rate its overall condition. With the numeric information gathered, we are beginning a sidewalk improvement program where sidewalks in the worst conditions will be fixed over the next few years. Going forward, we will continually inspect one-third of the city's sidewalks to keep the database and repairs up-to-date. **Our data shows that a portion of the sidewalk on your property has been identified as being in very poor or failing condition and is near the top of the priority list in requiring repairs.**

City ordinance states that residents are responsible for fixing sidewalks on their property (except those damaged by boulevard trees.) Per Section 7.16 Sidewalk and Retaining Wall Maintenance and Repair; Subd. 1 Primary Responsibility: *"It is the primary responsibility of the owner of property upon which there is abutting any sidewalk and/or retaining wall to keep and maintain such sidewalk and/or retaining wall in safe and serviceable condition."*

You may choose option (a) or (b) below to fix your sidewalk.

**a) The City can coordinate the work.** Staff will meet with you and provide an estimated cost of repairs. If you decide at that time to have the City handle the repair, your sidewalk will be added to next year's City Sidewalk Project that will begin after April 1st. You can pay the bill next year after the repair is completed or have it assessed to your property and pay it over a period of 10 years with interest.

**OR**

**b) You can coordinate the repair yourself.** Work must be done by a licensed contractor, and repairs must be completed by July 31, 2017. If you choose this route, you must contact the City's

Engineering Department first at 651-385-3674 so a staff member can meet with you and mark all areas of the sidewalk that need fixing. You'll also need to fill out an "Application for Work in City Right of Way" which stipulates that City staff must be on site to inspect the materials and pouring process to ensure the sidewalk abides by City code. You can pick up the form at the Public Works Department at 229 Tyler Road North, or find the application online at <http://www.red-wing.org>. (Click the "Government" button at the top, then the "Engineering" tab, then the "Engineering Permits, Applications, and Fees" link on the left side of the page.) After filling out the form, you can return it to the City of Red Wing Engineering Department via mail or in person at 229 Tyler Road North, Red Wing, MN 55066.

After the deadline of July 31, 2017 staff will re-inspect your sidewalk and if repairs have not been made, the City will fix the sidewalk and bill you, the property owner, for the work.

**Please call Jerry Plein at the Public Works Department at 651-385-3674 by November 1, 2016 to let us know how you would like to proceed.** We look forward to working together to build neighborhoods that are walkable for everyone. If you have any questions, feel free to contact me. Thank you.

Respectfully,

Lynn Nardinger  
Deputy Director, Public Works Department  
651-385-3654  
[Lynn.nardinger@ci.red-wing.mn.us](mailto:Lynn.nardinger@ci.red-wing.mn.us)

229 Tyler Road North  
Red Wing, MN 55066  
Website: [www.red-wing.org](http://www.red-wing.org)  
Phone: 651.385.3674  
Fax: 651.388.0243

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Red Wing, MN 55066  
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## State of Minnesota 2010 ADA Standards for Accessible Design

### Title II (i) (28 CFR 35.151)

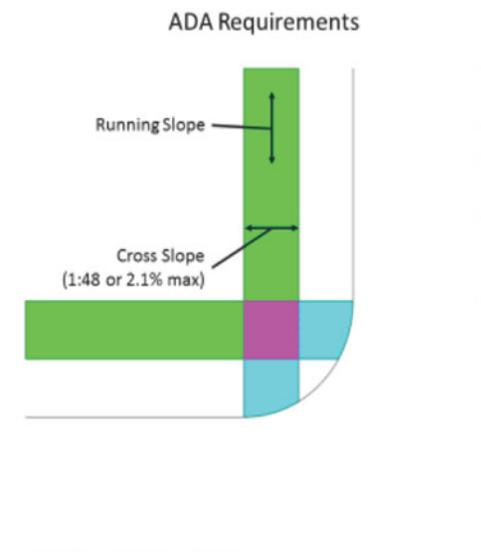
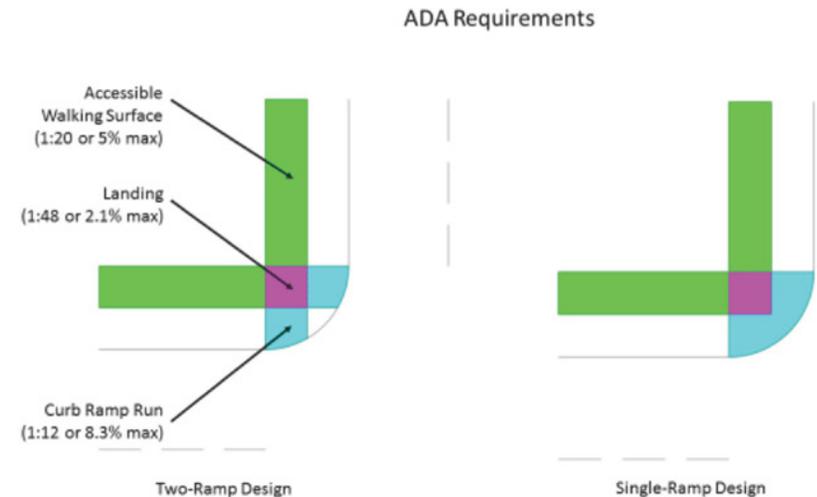
- “Newly constructed or altered streets, roads, and highways must contain curb ramps or other sloped areas at any intersection having curbs or other barriers to entry from a street level pedestrian walkway.”
- “Newly constructed or altered street level pedestrian walkways must contain curb ramps or other sloped areas at intersections to streets, roads, or highways.”
- Curb ramps and flared sides must be located so that they do not project into vehicular traffic lanes, parking spaces, or parking access aisles. Curb ramps and marked crossings shall be wholly contained within the markings, excluding flared sides.
- Diagonal or corner type curb ramps [...] shall have a clear space 48 inches minimum outside active traffic lanes of the roadway. Diagonal curb ramps provided at marked crossings shall provide the 48 inches minimum clear space within the markings.
- Any detectable warnings shall be 24 inches minimum in the direction of travel and extend the full width of the curb ramp or flush surface.
- Any detectable warning shall be located so the edge nearest the curb line is 6 inches to 8 inches from the curb line.

### State of Minnesota ADA Best Practices

- Follow Public Right-of-Way Access Guidelines, including installation of detectable warning surfaces.
- Follow MnDOT’s Pedestrian Ramp Details where appropriate. (City may tweak if necessary)
- Construct two-ramp design curb ramps where feasible.
- Orient detectable warning surfaces toward direction of crossing/sidewalk as much as possible.
- Utilize vertical curb behind sidewalk where buildings/walls are next to sidewalk and sidewalk grade needs to change significantly.
- Acquire right of way and/or easements where possible to make slopes work.
- When slopes won’t work due to grades, try to make the ramp runs and landing slopes work first.

*ADA requirements are complicated! Have an engineer design a solution if possible!*

### Federal ADA Requirements





**APPENDIX** **C**  
**STREET RECONSTRUCTION PROJECTS**

# APPENDIX C

## Criteria Sheet for Reconstruction Projects & Walkability Improvements in Developed Areas

Scoring Criteria for Engineer Reporting on Complete Streets Policy		
Roadway:	Segment:	
Traffic Volume (ADT):		
85th Percentile Speed:		
Criteria	Possible Score	Score
<b>Planning</b>		
1. Sidewalk segment is identified in the Red Wing Bicycle and Pedestrian Master Plan, Safe Routes to School Plan, or other adopted city plan.	10	
2. Sidewalk segment is within a designated historic district or downtown.	10	
PLANNING SUBTOTAL:		0
<b>Connectivity</b>		
3. Sidewalk segment is within the school walking zone (1 mile of an elementary school, 1.5 miles of a middle school, 2 miles of a high school, 0.25 miles of a community college or other educational institution).	15 for first school zone Add 5 per addl. zone	
4. Sidewalk segment is within a quarter mile of a neighborhood school bus stop AND outside of the school walking zone.	10	
5. Sidewalk segment is within a quarter mile of a city bus stop.	10	
6. Sidewalk segment is within a quarter mile of a city park or within a half mile of a community park.	5	
7. Sidewalk segment fills a gap in an existing sidewalk along a continuous roadway segment between two intersections.	10	
8. Sidewalk segment is on property that is being developed or redeveloped.	5	
9. Sidewalk segment is part of a street reconstruction project.	10	
10. Sidewalk segment is within a Commercial, High Density Residential, or Medium Density Residential existing land use district.	10	
CONNECTIVITY SUBTOTAL:		0
<b>Safety</b>		
11. Adjacent roadway traveling speed <i>(City to conduct 85th percentile speed study on roads with 30 mph speed limits)</i>	Less than 20 mph: 0 >20-25 mph: 5 >25-30 mph: 10 >30-35 mph: 15 >35-40 mph: 25 Above 40 mph: 35	
12. Adjacent roadway traffic volume	Less than 240 ADT: 0 240-600 ADT: 5 600-1200 ADT: 10 Each addl. 600 ADT: 5 Max Points: 45	
13. Sidewalk segment is inside of residential or commercial existing land use district and sidewalk exists only on one side of the street. <b>OR</b> Sidewalk segment is outside of residential or commercial existing land use district and no sidewalk exists on either side of the street.	5	
SAFETY SUBTOTAL:		0
<b>Point Adjustments</b>		
14. Sidewalk segment is on developed property & right-of-way is not publicly owned.	-10	
<b>SCORE</b>		
Sidewalk Segment Comparisons: Lowest Score of Analyzed Segments: 35      Highest Score of Analyzed Segments: 130		



**APPENDIX** **D**

**SIDEWALK CONNECTIONS ON VACANT LOTS**

# APPENDIX D

## Ordinance No. 83, Fourth Series

### AN ORDINANCE AMENDING CHAPTER 7, SECTION 7.06 OF STREET AND SIDEWALKS GENERALLY

THE CITY COUNCIL OF THE CITY OF RED WING DOES ORDAIN:

**SECTION 1:** Chapter 7, Section 7.06 of the City Code is hereby amended by deleting the ~~over struck~~ language and inserting the underlined language as shown below:

#### SECTION 7.06. CONSTRUCTION AND RECONSTRUCTION OF ROADWAY SURFACING, SIDEWALK, RETAINING WALLS, CURB AND GUTTER.

##### Subd. 1. Methods of Procedure.

- A. Abutting or affected property owners may contract for, construct or reconstruct roadway surfacing, sidewalk, retaining walls, or curb and gutter in accordance with this Section if advance payment is made therefor or arrangements for payment considered adequate by the City are completed in advance. For purposes of this Chapter, "retaining wall" refers to any retaining wall located on the public right of way abutting any private property.
- B. With or without petition by the methods set forth in the Local Improvement Code of Minnesota Statutes, presently beginning with Section 429.011, as the same may from time to time be amended.

- C. For Public sidewalks that are to be installed based on the provision listed in Subd. 5. A. below, the City shall notify the property owner that the owner has either three, five, or ten years from the date of notice to install the required public sidewalk according to City sidewalk specifications. The three, five, or ten year period is determined based on whether the undeveloped lot fronts on a Residential Collector Street, Residential Sub-Collector Street, or Residential Access Street as shown on the City's officially adopted Sidewalk Street Classification Map as referred to in Subd. 5.

1

A. below. If the property owner does not install the public sidewalk within the required time period, the City shall proceed to install the public sidewalk at the City's earliest convenience, and at the property owner's expense and invoice the cost of the sidewalk improvement to the property owner based on the City's fees set by Ordinance.

**Subd. 2.** Permit Required. It is a misdemeanor to construct or reconstruct a sidewalk, retaining wall, curb and gutter, driveway, or roadway surfacing in any street or other public property in the City without a right-of-way permit in writing from the City of Red Wing. Permits required by this subdivision shall be issued in accordance with Chapter 14.

**Subd. 3.** Specifications and Standards. All construction and reconstruction of roadway surfacing, sidewalk, retaining wall, and curb and gutter improvements, including curb cuts, shall use engineering best practices and be strictly in accordance with specifications and standards on file in the Engineering Department and open to inspection and copying there. Such specifications and standards may be amended from time to time by the City, but shall be uniformly enforced.

**Subd. 4.** Inspection. The City Engineer shall inspect such improvements as deemed necessary or advisable. Any work not done according to the applicable specifications and standards shall be removed and corrected at the expense of the permit holder. Any work done hereunder may be stopped by the City Engineer if found to be unsatisfactory or not in accordance with the specifications and standards, but this shall not place a continuing burden upon the City to inspect or supervise such work.

**Subd. 5.** Notwithstanding any provision herein to the contrary, the following provisions shall apply to the installation of public sidewalks, at the earliest of the following:

- A. At such time as public sidewalks have been installed on two-thirds of all the residential lots on any block face, public sidewalks shall be installed on the remaining undeveloped lots on that block face, based on the schedule listed below and pursuant to Subdivision 1 herein, at the property owner's expense. A block face is defined as all of the lots facing a public street from one intersection to the next intersection (this includes three and four way intersections and both sides of the street in the case of three way intersections). The schedule for installation of sidewalks on the undeveloped lots shall be as follows:
  1. Undeveloped lots that front on a Residential Collector Street as shown on the City officially adopted "Sidewalk Street Classification Map" shall be required to be installed within three years of two-thirds of the sidewalks on the block face having public sidewalks.
  2. Undeveloped lots that front on a Residential Sub-Collector Street as shown on the City officially adopted "Sidewalk Street Classification Map" shall be required to be installed within five years of two-thirds of the sidewalks on the block face having public sidewalks.
  3. Undeveloped lots that front on a Residential Access Street as shown on the City officially adopted "Sidewalk Street Classification Map" shall be required to be

2

installed within ten years of two-thirds of the sidewalks on the block face having public sidewalks.

~~B. At such time as public sidewalks have been installed on two-thirds of all the lots in any block, public sidewalks shall be installed on the remaining undeveloped lots in that block, pursuant to Subdivision 1 herein, at the property owner's expense.~~

~~C. Five years after the date on which the City Council accepted public street improvements on any plat, public sidewalks shall be installed on the remaining undeveloped lots in that plat, on which public sidewalks have not been installed, pursuant to Subdivision 1 herein, at the property owner's expense.~~

B. Whenever a building permit is issued for a new dwelling in zones R-1, R-2, RM-1, RM-2 or PUD (when public sidewalks are approved with the PUD approval), the property owner shall be responsible for the installation and expense of the public sidewalk; except that a new dwelling that has frontage on rural designed street section (ditches with no curb and gutter) shall not require sidewalk installation until such time as the street is reconstructed to an urban design (curb and gutter). The Building Official shall not issue a Certificate of Occupancy for the dwelling until the public sidewalk is installed; except that home construction that is completed after November 1<sup>st</sup> may obtain a temporary Certificate of Occupancy subject to completion of the public sidewalk not later than June 15<sup>th</sup> of the following year. In such cases, if the property owner who is responsible for the installation of the public sidewalk fails to install the public sidewalk by June 15<sup>th</sup> of the following year, the City shall proceed to install the sidewalk at the owner's expense and invoice the cost of the sidewalk improvement to the property owner based on the City's fees set by Ordinance. No variance of this requirement shall be approved except by the action of the City Council and upon the property owner showing that installation of the public sidewalk will not contribute to completing a public sidewalk network.

~~E. No variance to this Subdivision shall be granted without the approval of the City Council. No variance to this Subdivision shall be granted unless the property owner can show that strict enforcement of this Subdivision would result in unnecessary hardship not caused or created by the property owner.~~

~~F. If a property owner who is responsible for the installation of a sidewalk hereunder has failed to install the sidewalk within six months after issuance of the occupancy permit and has not obtained a variance, the Engineering Director may provide for the installation of the sidewalk, and costs may be charged back to the property.~~

**SECTION 2. Effective Date:** This Ordinance is effective 14 days following the publication of the ordinance in accordance with the City Charter.

Introduced the 14<sup>th</sup> day of March 2016

**Internal Process for Monitoring Vacant Lots and Notifying Residents Regarding Sidewalk Installation on Vacant Lots**

(Ordinance 83, Fourth Series)

Public Work, Engineering, and Planning staff met to discuss how the ordinance will be implemented. Below is the general process we expect to follow:

- The Engineering Department will be responsible to monitor and update the official Sidewalk Street Classification Map in case new streets need to be added to the map. The department will monitor the map and, as needed, provide recommendations to the City Council for review and approval of any changes.
- Every first quarter of the year, the Planning and Building Department will work with the GIS office to monitor and map sidewalks to determine whether block faces have met the 2/3s rule. The building official will notify the GIS office and Public Works of each new building permit that has been issued that resulted in a new sidewalk improvement. The Public Works Department will track other sidewalk improvements that have occurred that did not involve new home construction and notify the GIS office.
- Every first quarter of the year, the Planning and Building Department will notify property owners when the 2/3s rule has been met and let those property owners know about the timeframe for installing sidewalks. In addition, each first quarter of the year, the Planning and Building Department will notify property owners that met the 2/3s rule in previous years to let them know what the deadline is for sidewalk construction. In this way, if property is transferred, the new owner will be notified of the schedule.
- Every first quarter of the year, the Public Works Department will notify those property owners who decided not to install sidewalks prior to the timeframe deadline in order to let them know that the City will be constructing those sidewalks during the upcoming construction season and that they will be billed based on the City's fee schedule.
- Every first quarter of the year, the Planning and Building Department will update information so that the finance department has access to where the pending sidewalk improvements are located so that they can provide that information when requested.

# Gaps In Newer Neighborhoods: By the Numbers

Who Could Be Affected and Where?

These vacant lots without a sidewalk are on street sections where 2/3 of the lots already have sidewalks. Timeframe suggestion is based on the category of street. (Lots are also highlighted at right.)

**Install a Sidewalk Within 3 Years (High-use/Collector Streets)**

- Aspen Avenue 6 lots
- Hallquist Avenue 2 lots  
*(Hallquist section is only the north side of road between Pine Ridge Blvd. & Southview Ridge)*

**TOTAL: 8 lots within 3 years**

**Install a Sidewalk Within 5 Years (Local-use Streets)**

- Sunny Meadow Lane 3 lots
- Frances Avenue 2 lots

**TOTAL: 5 lots within 5 years**

**Install a Sidewalk Within 10 years (Low-use/Access Streets)**

- Hillside Drive 4 lots
- Gadient Place 3 lots
- Snowbird Lane 1 lot
- Pine Ridge Boulevard 15 lots
- Pineview Court 2 lots
- Hidden Knoll 1 lot
- Goldfinch Lane 2 lots
- Chickadee Court 3 lots
- High Park Street 1 lot
- Neal Street 5 lots

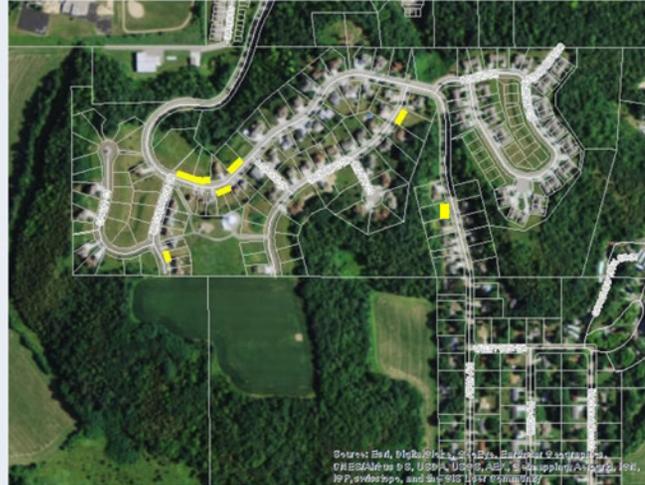
**TOTAL 37 lots within 10 years**

**Total lots filled between 3 and 10 years: 50 lots**

\* An additional 44 lots would be filled sometime after 10 years when the streets on the City-approved maps (such as Kosec Drive, Hewitt Blvd., and Briarwood) eventually have more houses built and fall into the 2/3 category.

**Charlson Crest Neighborhood**

6 lots — within 3 years  
2 lots — within 10 years



**Pine Ridge Boulevard & Pineview Court**

17 lots — within 10 years



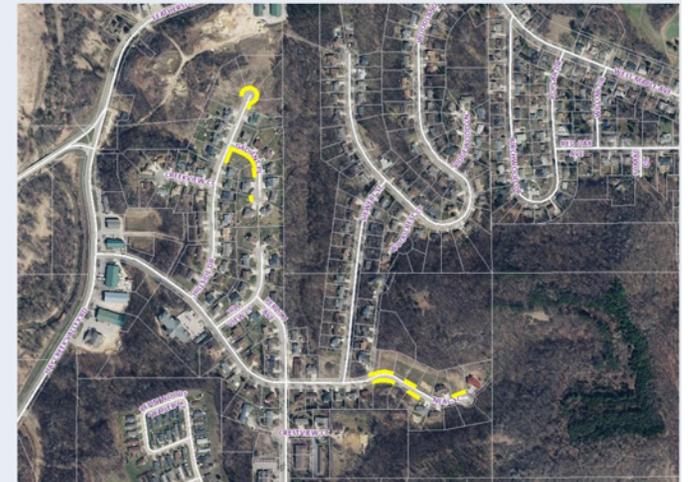
**Hallquist / Sunny Meadow / Frances**

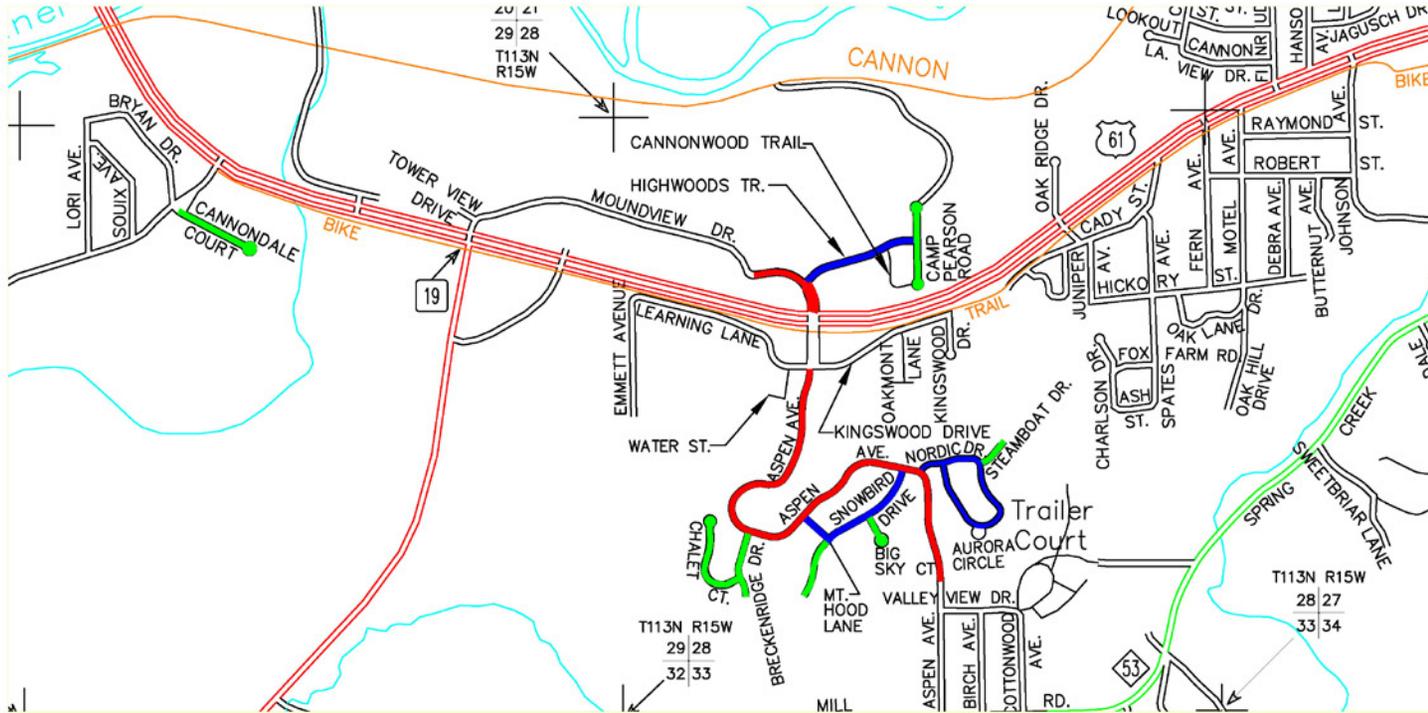
2 lots within 3 years; 5 lots within 5 years  
4 lots within 10 years



**Neal Street / Gadient Place / Hillside Drive**

12 lots within 10 years





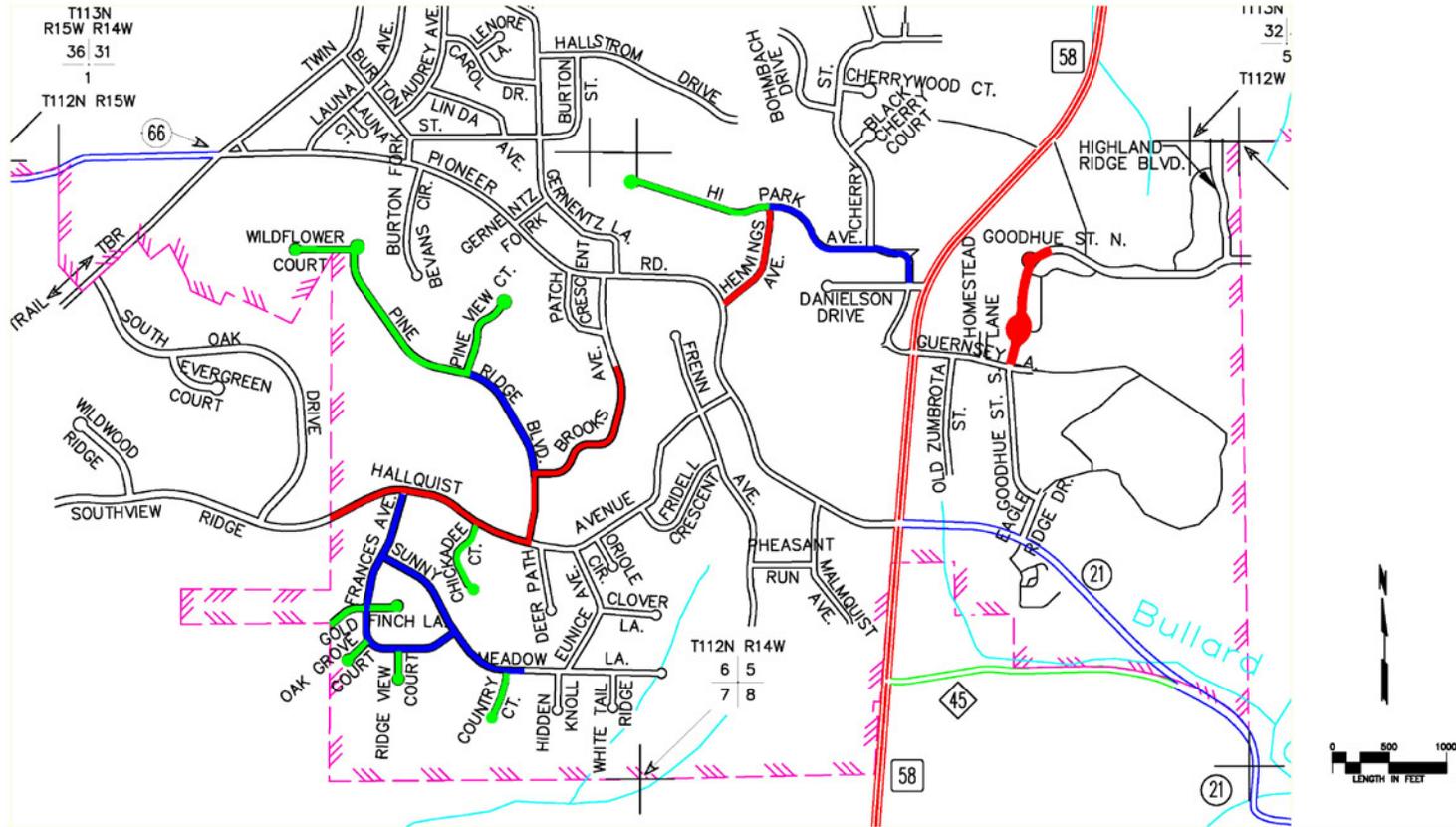
**LEGEND:**

- RESIDENTIAL COLLECTOR - 3 YEARS** ———
- RESIDENTIAL SUB-COLLECTOR - 5 YEARS** ———
- RESIDENTIAL ACCESS - 10 YEARS** ———



TITLE:  
**SIDEWALK STREET  
CLASSIFICATION MAP**  
Sheet No. 1 of 3 Sheets

# APPENDIX D



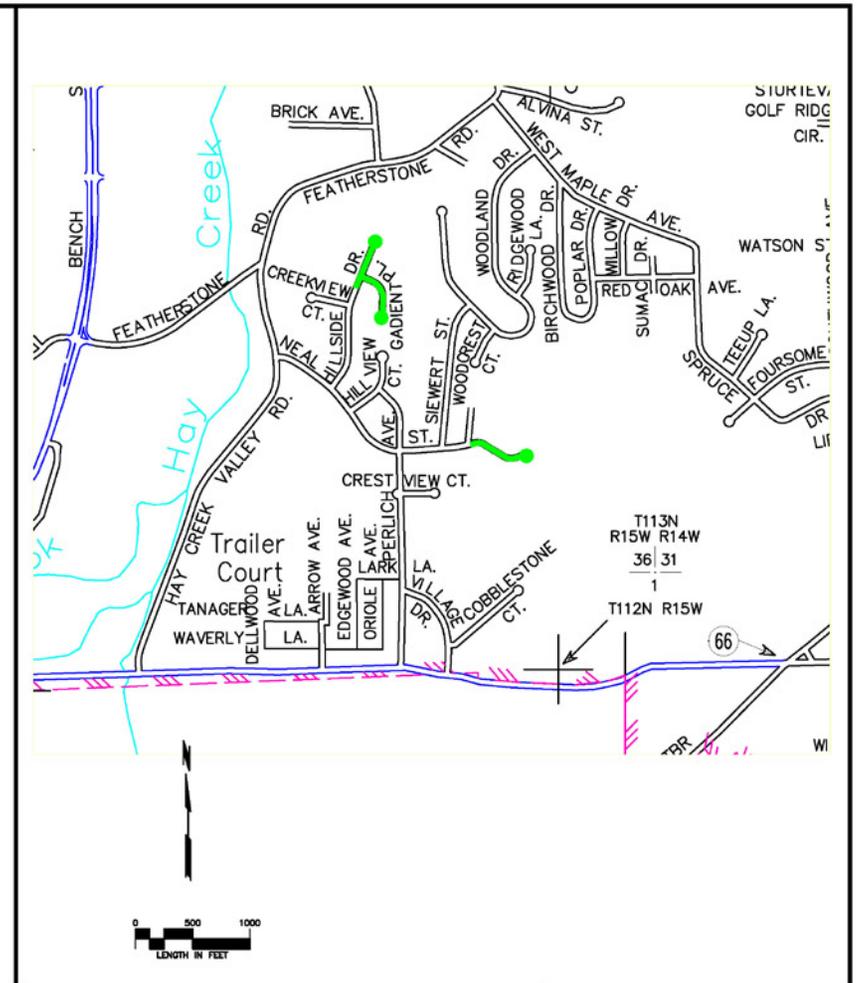
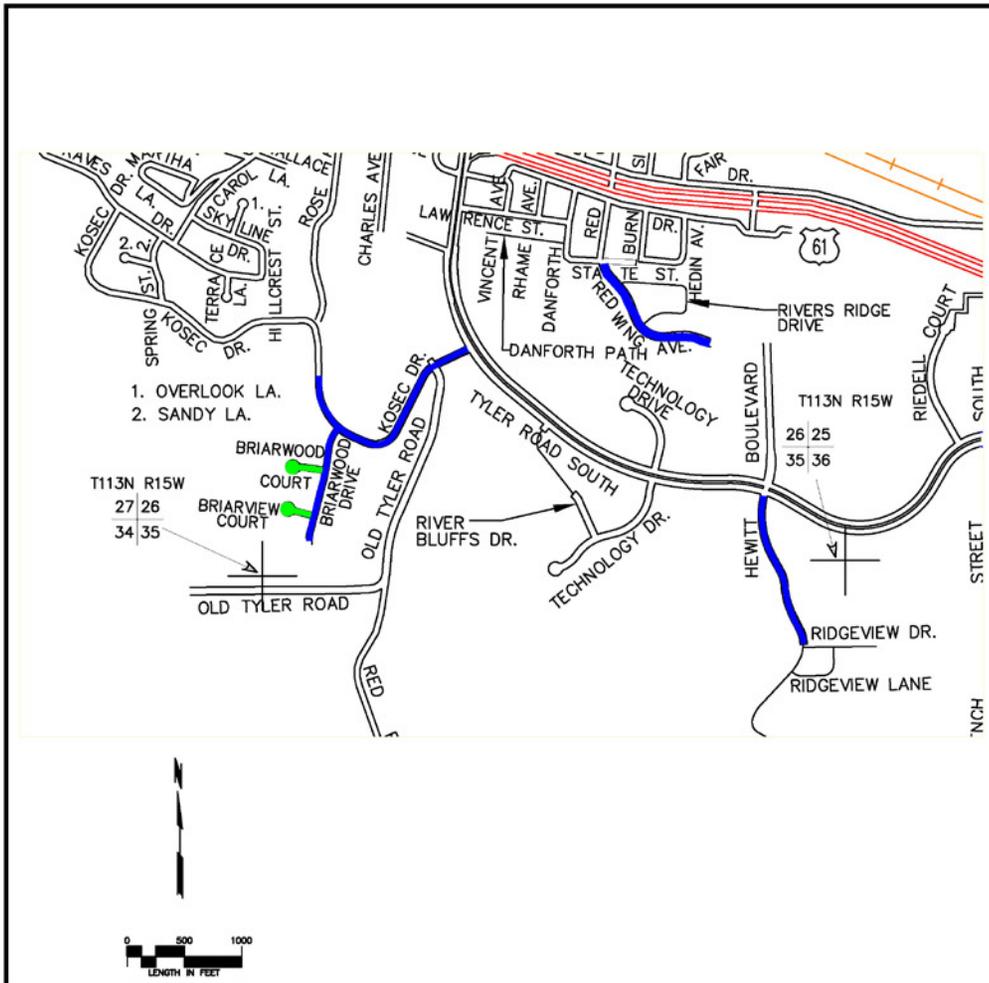
**LEGEND:**

- RESIDENTIAL COLLECTOR - 3 YEARS** ———
- RESIDENTIAL SUB-COLLECTOR - 5 YEARS** ———
- RESIDENTIAL ACCESS - 10 YEARS** ———



TITLE:  
**SIDEWALK STREET  
CLASSIFICATION MAP**

Sheet No. 2 of 3 Sheets



**LEGEND:**

**RESIDENTIAL COLLECTOR - 3 YEARS**



**RESIDENTIAL SUB-COLLECTOR - 5 YEARS**



**RESIDENTIAL ACCESS - 10 YEARS**



TITLE:  
**SIDEWALK STREET CLASSIFICATION MAP**

Sheet No. 3 of 3 Sheets

# APPENDIX D



September 16, 2015

Address here  
Three lines long  
Three lines long

Dear Resident,

For the past 18 months, the Mayor's Task Force on Streets and Sidewalks has been surveying, researching, and learning more about Red Wing's sidewalk infrastructure. Part of that work includes looking at neighborhoods where some lots have sidewalks and others do not. **You have received the enclosed sidewalk survey because you live in a neighborhood that has gaps in its sidewalk system**, and as a part of our ongoing research we are asking for your input as a resident of this neighborhood. **Results will be used by the Mayor's Task Force as it weighs future sidewalk recommendations to City Council.** More surveys may follow.

**This survey will take 3 to 5 minutes to fill out. Your answers will be kept private and grouped with the answers of others.** After you complete the survey, please mail it back in the enclosed self-addressed, stamped envelope **by Saturday, September 26.**

Survey results will be made available on the City of Red Wing's web site at [www.red-wing.org](http://www.red-wing.org) the week of October 18th. If you are interested in learning more about the Mayor's Task Force on Streets and Sidewalks and its work, please go to the City's web site and click the Mayor's Task Force button.

I am always available to listen more to your opinions regarding this issue. If you'd like to contact me directly, please email me at [dan.bender@ci.red-wing.mn.us](mailto:dan.bender@ci.red-wing.mn.us).

Thank you for your time.

Sincerely,

Dan Bender  
City of Red Wing Mayor



## Sidewalk Survey: Frances-Sunny Meadow Area

The Mayor's Task Force is interested in your opinions because you live in an area with sidewalk gaps (where some lots have sidewalks and others don't). Results from this survey will help inform the Task Force in their ongoing sidewalk research and help inform possible recommendations they make to City Council. **Your answers will be kept private and grouped with the answers of others. Please return this survey in the enclosed self-addressed stamped envelope by Saturday, September 26.**

1. Please circle how much you agree or disagree with the following statement:

**It is important to me to have connected sidewalks in my neighborhood.**

Strongly agree      Agree      Disagree      Strongly Disagree

Explain why you chose this answer: *(Optional)* \_\_\_\_\_

2. Since 1989, sidewalks get installed on lots as houses are built. In some neighborhoods, this leaves gaps in the sidewalk system. Please circle how much you agree or disagree with the following statement:

**It's fine to have sidewalk gaps in my neighborhood until all the houses have been built.**

Strongly agree      Agree      Disagree      Strongly Disagree

Explain why you chose this answer: *(Optional)* \_\_\_\_\_

3. Please circle how much you agree or disagree with the following statement:

**If sidewalk gaps remain for a long time, a process should be in place so those sidewalk gaps can eventually be filled.**

Strongly agree      Agree      Disagree      Strongly Disagree

Explain why you chose this answer: *(Optional)* \_\_\_\_\_

**Examples of sidewalk gaps are marked in red on the map shown on the back of this survey.** If you would like to share opinions or ask questions, contact Mayor Dan Bender at [dan.bender@ci.red-wing.mn.us](mailto:dan.bender@ci.red-wing.mn.us). For details, visit the City of Red Wing's website at [www.red-wing.org](http://www.red-wing.org) and click the Mayor's Task Force button on the right side of the screen. **Surveys on more sidewalk issues in your area will follow in the future.**

# Sidewalk Gaps In Newer Neighborhoods

## In Residents' Own Words



*Quotes are a representative sampling of resident comments from the first three neighborhoods surveyed: Charlson Crest; Pine Ridge Boulevard/Pineview Court; and Sunny Meadow/Frances/(west) Hallquist area*

## APPENDIX D

### Developers' Concerns

The following list outlines local developers' concerns as they pertain to the building sidewalks on vacant lots to connect the sidewalk network in neighborhoods with sidewalk gaps. Mayor's Task Force facilitator Michelle Leise interviewed local developers Tracy Gadiant, Larry Kull, and Paul Siewert for this collective list. They have reviewed, edited, and approved it.

- \* **Sidewalks may break during construction** even if a specific area is designated for construction vehicles; this can include additional disposal fees. Sidewalks can cost around \$2,500 for a 100-foot long segment (average measurement for these neighborhoods).
- \* **Snow maintenance costs money.**
- \* **Summer maintenance** can also be an issue because without it, long grasses and bugs encroaching on the sidewalk deter people from using it so it's a sidewalk people don't want to use. Paying for mowing, even a few times a season, will cost money.
- \* **The location and depth of utility work** that happens during construction (electrical, phone, cable, sewer, water) sometimes requires that portions of the sidewalk be dug up. This creates a color-match problem between the old and new concrete of the sidewalk, and new homeowners will not accept that.
- \* **Safety.** The control valve for the water supply (the "curb stop") is sometimes too close to the sidewalk. When excavators come to work on it, they need to dig a big enough hole around it to ensure workers' safety. Sometimes a sidewalk needs to be dug up to provide enough space for the hole.
- \* **No one knows where the driveway will be** until the house is built so that can't be done ahead of time.
- \* **A sidewalk could potentially be installed at the wrong height because of grading** that's done during construction.
- \* New homeowners sometimes want special driveways in a specific color; **then the sidewalk doesn't match their color or style** so they want the initial sidewalk replaced.

# SIDEWALK PLANNING PUBLIC OPEN HOUSE

**Wednesday, January 13, 2016  
4:30–6:30 pm**

**Public Library, Foot Room  
225 East Avenue, Red Wing**

**Stations will be set up to share resident survey results and data, and collect feedback on multiple issues: gaps in the sidewalk network, potential future sidewalks, accessibility and safety issues, and more.**

**City staff will be available to listen and answer questions. No formal presentation. Comment boxes available.**

**Can't attend? Email your opinions and ideas to Mayor Dan Bender at:  
[dan.bender@ci.red-wing.mn.us](mailto:dan.bender@ci.red-wing.mn.us)**

**Visit [www.red-wing.org](http://www.red-wing.org) for more details and survey results.**

**Hosted by the City of Red Wing, Live Healthy Red Wing, and the Mayor's Task Force on Streets and Sidewalks**



**APPENDIX** **E**  
**SUBDIVISION CODE**

## Sidewalk Installation with Streets Other Improvements in New Subdivisions

Staff conducted research into other communities that require sidewalks be installed with streets and other improvements. Generally, cities that require sidewalks be installed with other improvements leave it up to builders and contractors to minimize damage to sidewalks during construction. All cities that were interviewed do not offer waivers in order to avoid setting a precedent in future subdivisions.

### Duluth, MN

- Requires sidewalks be installed with other improvements.
- Damage Mitigation -Typically driveway aprons/curb cuts are installed with streets and use thicker sidewalks in those locations.
- Winter Maintenance - Snow removal belongs to property owners. There are usually Homeowners' Associations for subdivisions that take care of it.
- Waivers - Engineering doesn't recommend waivers or delays to avoid setting a precedent.

### Northfield, MN

- Requires sidewalks to be installed with other improvements.
- Damage Mitigation – nothing formal, but certificate of occupancy depends on approval of improvements including sidewalks, sewers, etc. If sidewalks are damaged, a certificate is not given for the home.
- Winter Maintenance – System is complaint-based, so the city doesn't inspect sidewalks as often.
- Waivers – City doesn't issue waivers, but developers ask for them.

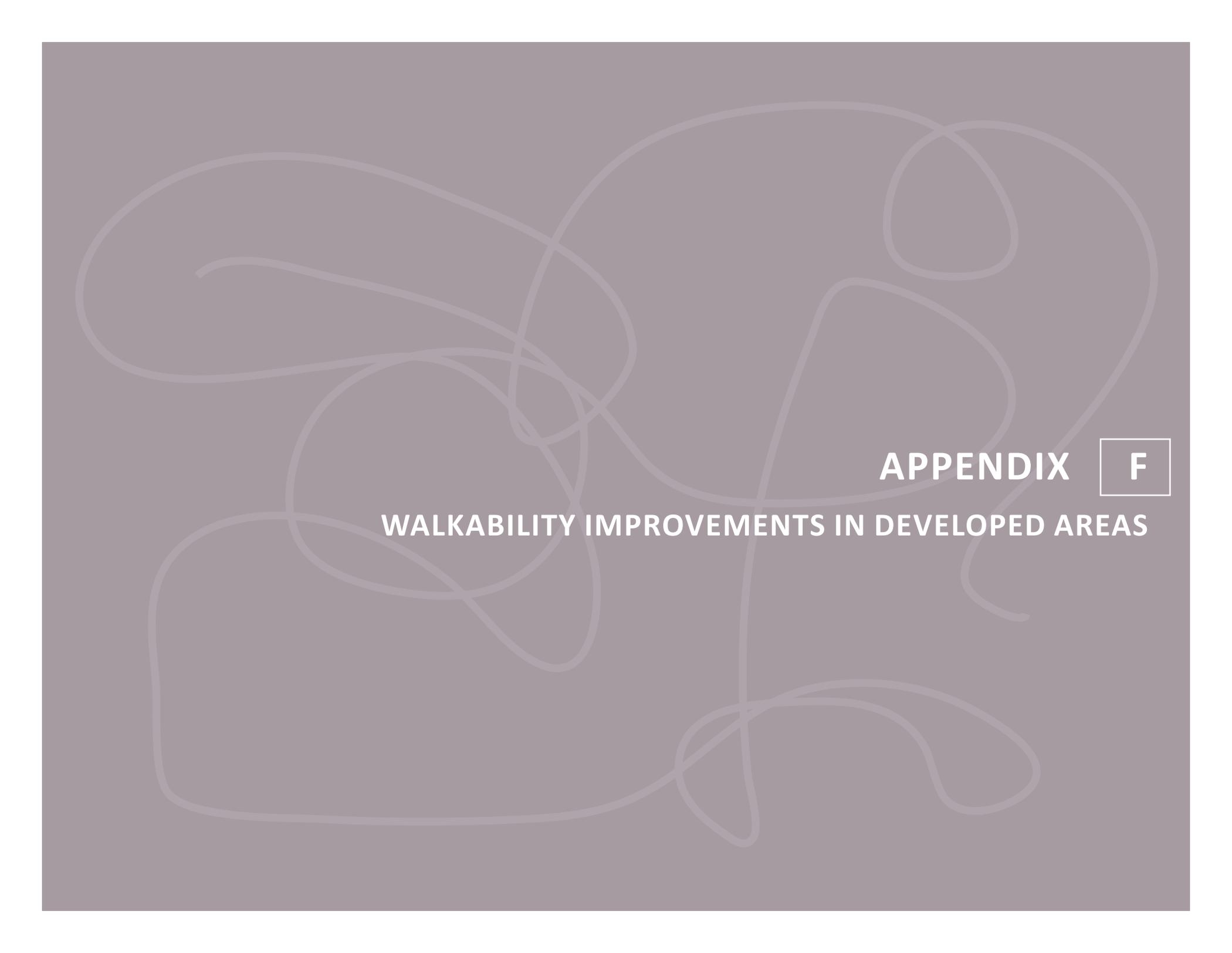
### Hastings, MN

- Requires sidewalks as called for by the city sidewalk plan.
- Damage Mitigation – Sidewalks are reconstructed to driveway specifications if a driveway is put in after the fact.
- Winter Maintenance – Snow removal is up to the developer. Snow removal is complaint-based.
- Waivers – Not all streets get sidewalks, so waivers are not given where required.

### Cottage Grove

- Developers install sidewalk during street construction
- 6" thick sidewalk prevents most damage
- Building permits include escrow for sidewalk
- Development agreement: developers maintain infrastructure until city takes over ownership (80% of homes)

Red Wing is more forward-thinking and pedestrian-friendly in that sidewalks are inspected for snow removal. Other cities are solely complaint-based with regards to snow removal. It's worth noting that most recently developed neighborhoods in Red Wing are lower on the priority scale for snow inspections and don't get checked as often.



**APPENDIX**

**F**

**WALKABILITY IMPROVEMENTS IN DEVELOPED AREAS**

# APPENDIX F

## Criteria Sheet for Reconstruction Projects & Walkability Improvements in Developed Areas

Scoring Criteria for Engineer Reporting on Complete Streets Policy		
Roadway:	Segment:	
Traffic Volume (ADT):		
85th Percentile Speed:		
Criteria	Possible Score	Score
<b>Planning</b>		
1. Sidewalk segment is identified in the Red Wing Bicycle and Pedestrian Master Plan, Safe Routes to School Plan, or other adopted city plan.	10	
2. Sidewalk segment is within a designated historic district or downtown.	10	
PLANNING SUBTOTAL:		0
<b>Connectivity</b>		
3. Sidewalk segment is within the school walking zone (1 mile of an elementary school, 1.5 miles of a middle school, 2 miles of a high school, 0.25 miles of a community college or other educational institution).	15 for first school zone Add 5 per addl. zone	
4. Sidewalk segment is within a quarter mile of a neighborhood school bus stop AND outside of the school walking zone.	10	
5. Sidewalk segment is within a quarter mile of a city bus stop.	10	
6. Sidewalk segment is within a quarter mile of a city park or within a half mile of a community park.	5	
7. Sidewalk segment fills a gap in an existing sidewalk along a continuous roadway segment between two intersections.	10	
8. Sidewalk segment is on property that is being developed or redeveloped.	5	
9. Sidewalk segment is part of a street reconstruction project.	10	
10. Sidewalk segment is within a Commercial, High Density Residential, or Medium Density Residential existing land use district.	10	
CONNECTIVITY SUBTOTAL:		0
<b>Safety</b>		
11. Adjacent roadway traveling speed <i>(City to conduct 85th percentile speed study on roads with 30 mph speed limits)</i>	Less than 20 mph: 0 >20-25 mph: 5 >25-30 mph: 10 >30-35 mph: 15 >35-40 mph: 25 Above 40 mph: 35	
12. Adjacent roadway traffic volume	Less than 240 ADT: 0 240-600 ADT: 5 600-1200 ADT: 10 Each addl. 600 ADT: 5 Max Points: 45	
13. Sidewalk segment is inside of residential or commercial existing land use district and sidewalk exists only on one side of the street. <b>OR</b> Sidewalk segment is outside of residential or commercial existing land use district and no sidewalk exists on either side of the street.	5	
SAFETY SUBTOTAL:		0
<b>Point Adjustments</b>		
14. Sidewalk segment is on developed property & right-of-way is not publicly owned.	-10	
<b>SCORE</b>		
Sidewalk Segment Comparisons:		
Lowest Score of Analyzed Segments: 35      Highest Score of Analyzed Segments: 130		

**Street Scoring Sheet: Potential Future Sidewalks and Reconstruction Projects**

Data collected 2016

Street	Segment	Score
West Avenue	Between Hawthorne Street and Sanford Street	125
Pioneer Road	Between Perlich Avenue and Hay Creek Valley Road	115
Bush Street	Between 17th Street and South Park Street	110
Old West Main Street	Behind pottery building	85
Launa Avenue	Between Pioneer Road and Burton Street	75
Malmquist Avenue	Between Pioneer road and Pheasant Run	75
Neal Street	Between Hay Creek Valley Road and Hillside Drive	70
North Service Drive	Near Arby's	70
Pheasant Run	Between Malmquist Avenue and Frenn Avenue	70
Tile Drive	North of Brick Avenue	70
Bench Street	Between TH 61 and Old West Main Street	65
South Park Street	Between 13th Street and 17th Street	65
South Service Drive	Near Pennzoil	65
Carol Lane	Between Forrest Lane and Martha Lane	60
Hallquist Avenue	At 2380 Hallquist Avenue	60
Hallstrom Drive	Near Greenhouse	60
Eunice Avenue	Between Hallquist Avenue and Clover Lane	55
Hennings Avenue	Between Pioneer Road and Hi Park Avenue	55
Bush Street	Between 22nd Street and 23rd Street	50
Central Park Street	Between 13th Street and 16th Street	50
South Park Street	Between 17th Street and 18th Street	50
Grace Street	West of Central Avenue	45
Sturtevant Street	West of Prairia Street	45
Alvina Street	East of Featherstone Road	35

Denotes Prior Street Reconstruction Project

January 1, 2016



THREE LINES  
FOR RESIDENT  
ADDRESS GOES HERE

Dear Resident,

The City of Red Wing and the Mayor's Task Force on Streets and Sidewalks are gathering information on several local streets that have been identified as gap areas in the sidewalk network. These streets were noted in Red Wing's Bike-Pedestrian Master Plan, approved by City Council in 2011. In order to study each street further, a procedure has been developed that includes collecting traffic data, analyzing research, holding Open Houses to hear from the public, and surveying neighborhoods to gather the opinions of residents and landowners.

**The section of Hallquist Avenue between Pioneer Road and Pine Ridge Boulevard, highlighted on the map on the back of this letter, is one of the street sections identified to be researched at this time.** A traffic study completed in October showed an average of 900 vehicles per day drive along this section of Hallquist, with 85% of the drivers traveling an average of 34.5 miles per hour.

You have received the enclosed sidewalk survey because you live on or near this area, and your opinions are valued and important to this process. **The survey will take 3 to 5 minutes to fill out. Your anonymous answers will be kept private and grouped with the answers of others.** After you complete the survey, please mail it back in the enclosed self-addressed, stamped envelope, postmarked **by Saturday, January 9.** Results will be compiled and shared with the public, City Council, and City staff, and also considered by the Mayor's Task Force as it weighs its future sidewalk recommendations to City Council.

**You are also invited to attend a Sidewalk Open House on Wednesday, January 13 from 4:30 to 6:30 p.m. in the Foot Room of the Public Library.** The Open House will offer an informal setting for residents to learn about and give feedback on a variety of sidewalk issues, including the ongoing research in sidewalk gap areas. Survey results from your neighborhood will be available at the Open House and also online at [www.red-wing.org](http://www.red-wing.org) starting January 15th.

If you are interested in learning more about the Mayor's Task Force on Streets and Sidewalks and its work, please go to [www.red-wing.org](http://www.red-wing.org) and click the Mayor's Task Force button. **I am always available to listen to your opinions regarding this issue. If you'd like to contact me directly, please email me at [dan.bender@ci.red-wing.mn.us](mailto:dan.bender@ci.red-wing.mn.us).** Thank you for your time.

Sincerely,

A handwritten signature in black ink that reads "Daniel D. Bender".

Dan Bender  
City of Red Wing Mayor



## Sidewalk Survey: Hallquist Avenue

You are receiving this survey because you live on or near a street section where traffic data and resident input are being collected (*Hallquist Avenue between Pioneer Road and Pine Ridge Boulevard as shown on the enclosed map*). Your opinions are important to this process. **Your answers will be kept private and grouped with the answers of others.** (See attached letter for details). **Please return survey in the enclosed self-addressed stamped envelope, postmarked by Saturday, January 9.**

1. If a sidewalk were being considered on the section of Hallquist Avenue between Pioneer Road and Pine Ridge Boulevard (the area is highlighted on the enclosed map), what statements below would closely resemble your opinions? (CHECK ALL THAT APPLY.)

- Sidewalks along this section would increase safety for pedestrians of all ages and abilities.
- The street is already safe for everyone and doesn't need sidewalks.
- I (or my family) would walk more frequently if there were sidewalks along that section.
- People would not use the sidewalk.
- When I'm driving, I would feel better having pedestrians on a sidewalk along that area.
- Putting in a sidewalk is not worth removing and replanting trees and landscaping.
- Sidewalks would make my property more appealing to prospective buyers.
- Adding sidewalks would cost too much money for affected homeowners.
- A sidewalk would make winter walking in this area safer.
- I don't like that homeowners would have to shovel and maintain their sidewalks.
- Sidewalk alternatives would be better, such as painted lines on both sides of the street.
- Sidewalk alternatives are a step but not a safety substitute for sidewalks.

Other: \_\_\_\_\_  
 \_\_\_\_\_  
 \_\_\_\_\_  
 \_\_\_\_\_  
 \_\_\_\_\_

Turn over to complete survey

2. Do you currently walk in your neighborhood?

- Yes                       No

3. Would you walk more in your neighborhood if a sidewalk were built along the highlighted section of Hallquist Avenue as shown on the enclosed map?

- Yes                       No

Explain why you chose this answer: \_\_\_\_\_  
 \_\_\_\_\_  
 \_\_\_\_\_

4. Circle how much you agree or disagree with the following statement:

**“Having sidewalks on the highlighted section of Hallquist Avenue would make it better for students walking or biking to their school or their bus stop.”**

- Strongly Agree              Agree              Disagree              Strongly Disagree

*(Note: This section of Hallquist Avenue has bus stops for kindergartners through 4<sup>th</sup> graders and is in the school walk zone for 5<sup>th</sup> through 12<sup>th</sup> graders.)*

Explain why you chose this answer: \_\_\_\_\_  
 \_\_\_\_\_  
 \_\_\_\_\_

5. To what extent do you support adding sidewalks along the section of Hallquist Avenue between Pioneer Road and Pine Ridge Boulevard as highlighted on the enclosed map?

- Strongly Support              Support              Oppose              Strongly Oppose

6. Is there anything else you would like the City of RW and the Mayor's Task Force to know?

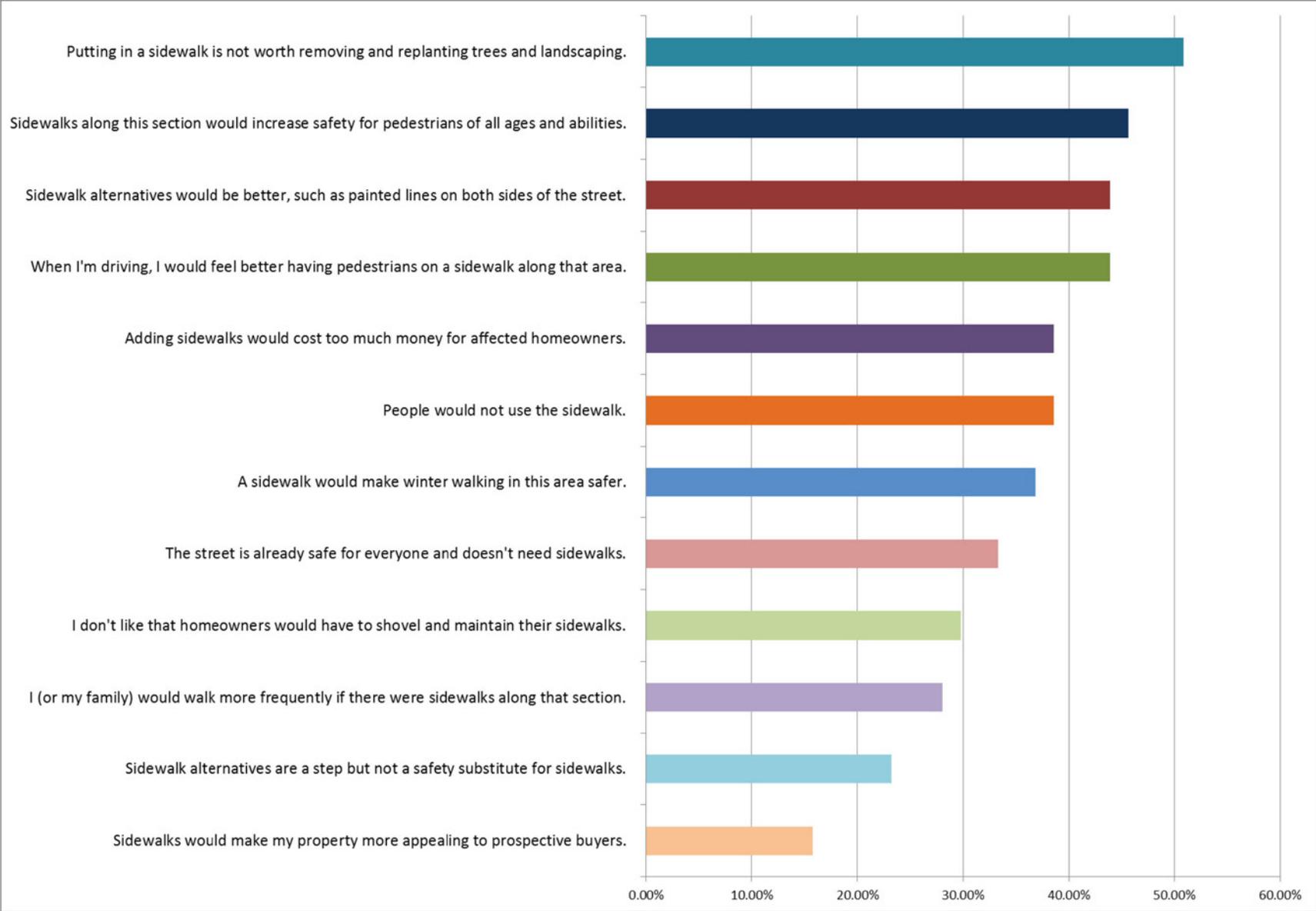
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Thank you!

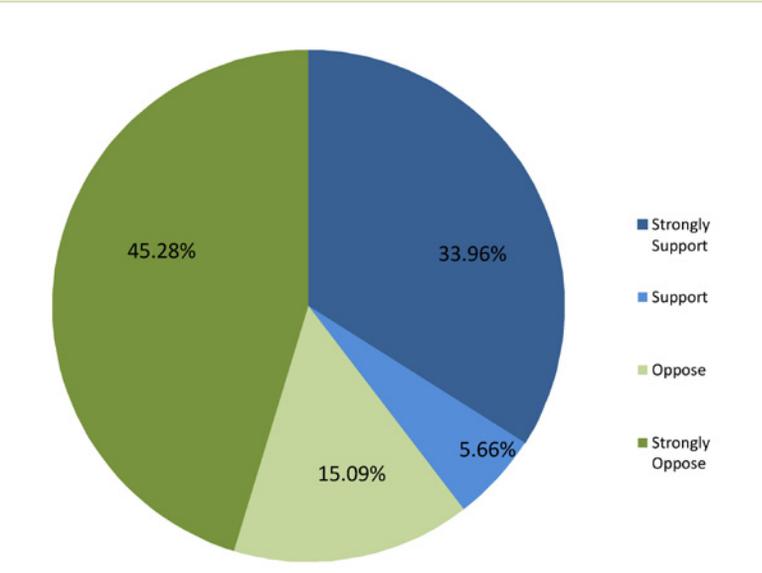
*Note: The area of the Hallquist Avenue neighborhood between Pine Ridge Boulevard and Southview Ridge was surveyed in September.*

APPENDIX F

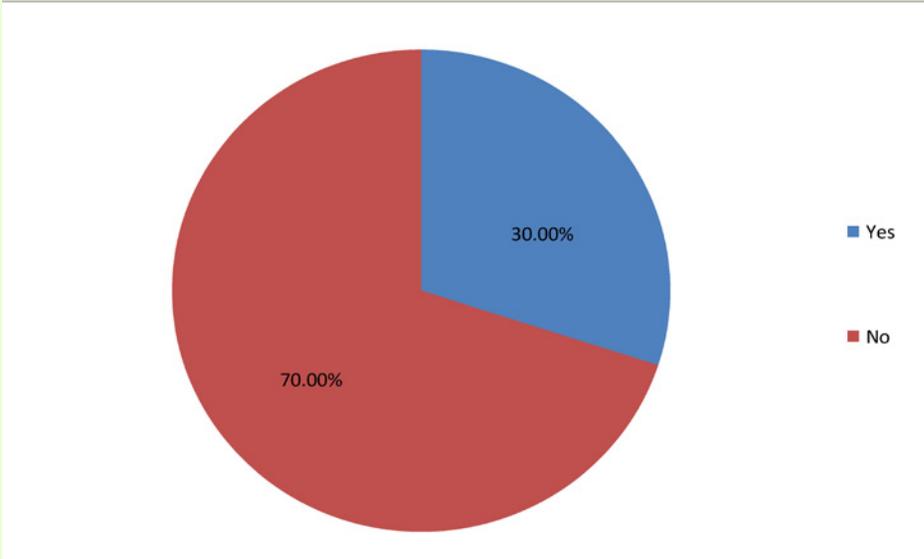
**Responses from Neighborhood of Frenn, Eunice, and Hallquist between Pioneer & Pine Ridge Blvd.**  
**Sidewalk Survey Taken January 2016**



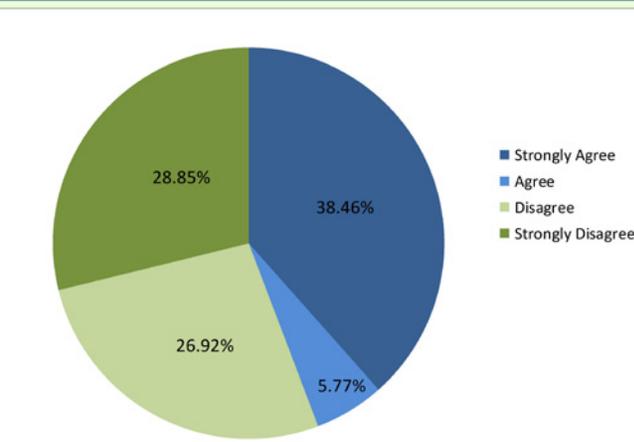
To what extent do you support adding sidewalks along the section of Hallquist Avenue between Pioneer Road and Pine Ridge Boulevard?



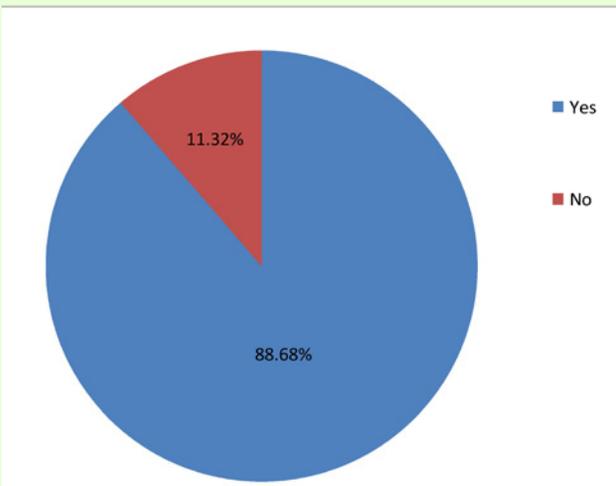
Would you walk more in your neighborhood if a sidewalk were built along the highlighted section of Hallquist Avenue as shown on the enclosed map?



"Having sidewalks on the highlighted section of Hallquist Avenue would make it better for students walking or biking to their school or their bus stop."

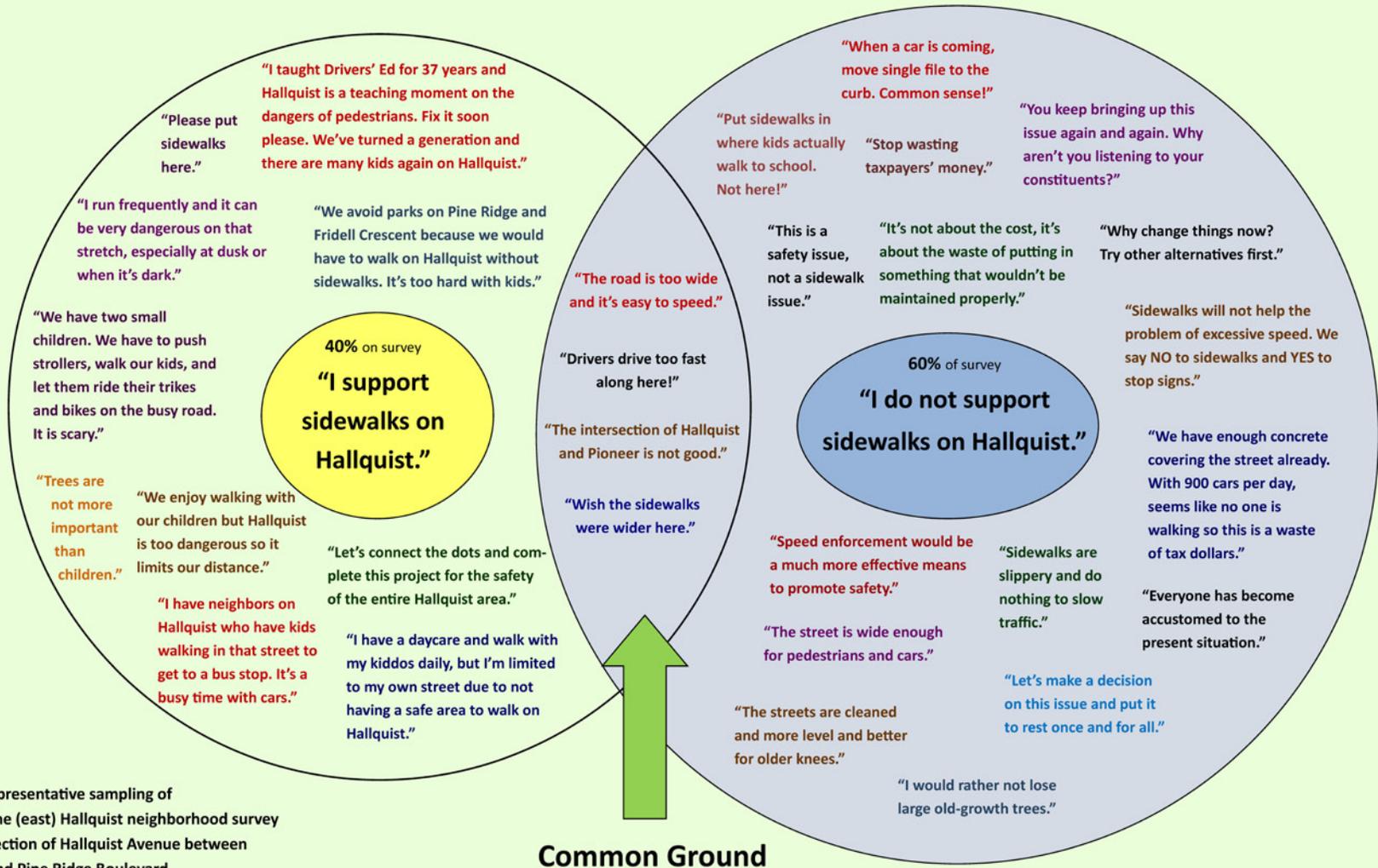


Do you currently walk in your neighborhood?

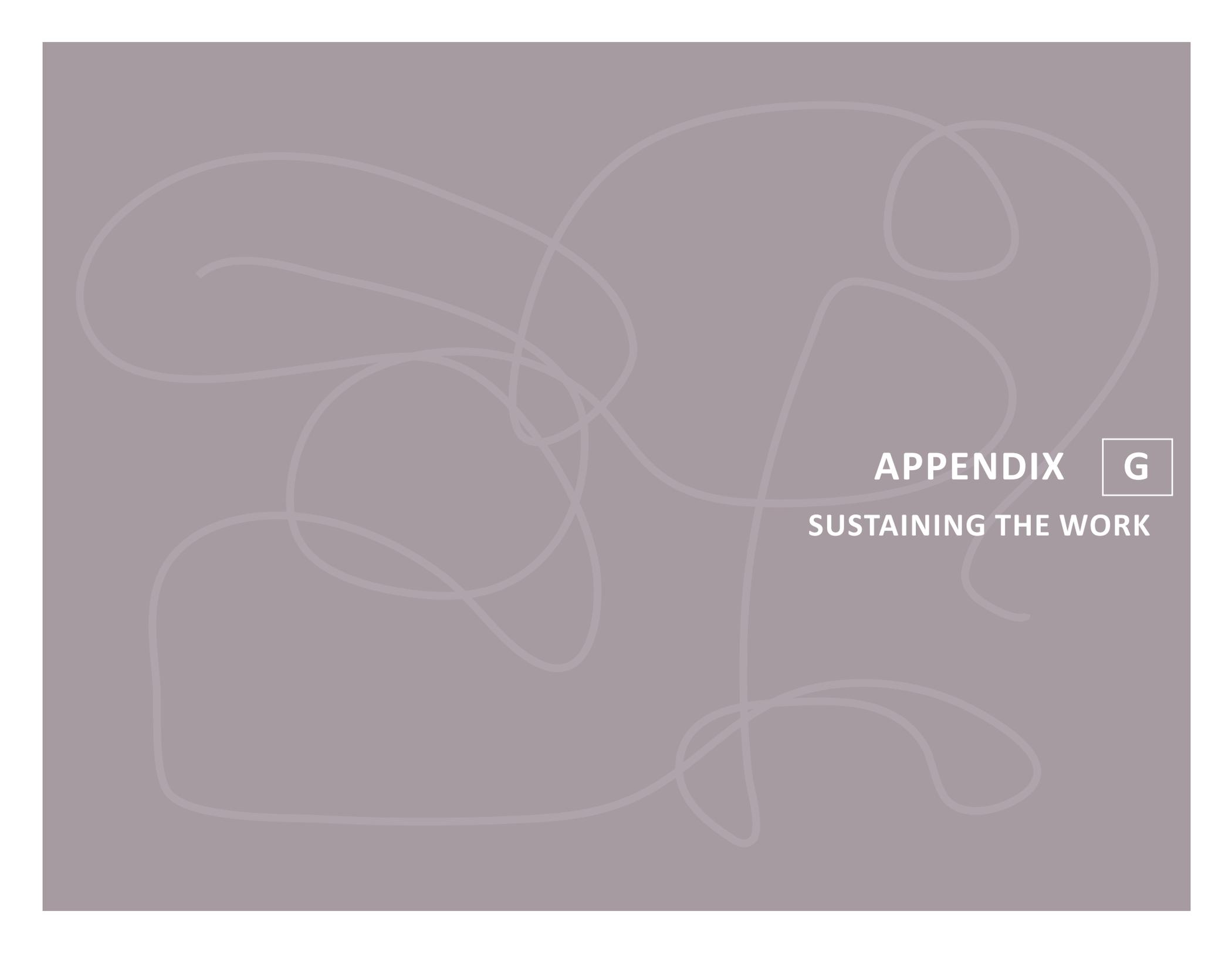


# Areas of Potential Future Improvements

## In Residents' Own Words / Area #1: Hallquist-Eunice-Frenn Area



Quotes are a representative sampling of comments on the (east) Hallquist neighborhood survey regarding the section of Hallquist Avenue between Pioneer Road and Pine Ridge Boulevard.



**APPENDIX** **G**  
**SUSTAINING THE WORK**

## The HEEAP Sheet

**WHAT is Health, Equity and Excellence in All Policies [HEEAP]?** A comprehensive, collaborative approach to developing plans and policies that takes into account the equity and well-being of all residents early on and throughout the decision-making process. **WHY do this?** Because plans and policies that impact the conditions in which people are born, live, learn, work, play, and age have an enormous effect on how healthy people are and how their community thrives. Policy areas include sectors such as transportation, housing, public safety, parks, economic development, air and water, and more. Government has the ability to positively and profoundly affect the health, equity, and well-being of all residents and the community.

**Questions to Guide the Development of a Policy, Plan, or Procedure:**

- 1) What is the policy, plan, or procedure, under consideration?**
  - What are the desired results (within community) and outcomes (within our internal City of RW)?*
  - What does this proposal have the ability to impact?*
  - What are best practices? What do other communities do?*
  
- 2) What is the data we have regarding this proposal? What does that data tell us?**
  - Will this impact a specific geographic area, neighborhood, or demographic?*
  - Do we have data on those areas or populations most affected?*
  - Are there gaps in the data we have? If so, how could we get better or updated data?*
  
- 3) How have different populations been engaged? Are there opportunities to expand that?**
  - How have we engaged and involved people most affected or most concerned?*
  
- 4) Who will benefit from the proposal? Who will be burdened?**
  - Are there potential unintended negative consequences & what are strategies to decrease those?*
  - Are there ways to maximize positive impact?*
  - Could we partner with community stakeholders for longer-term positive impact?*
  
- 5) What is the plan for implementation?**
  - Is the implementation plan realistic and properly resourced with funding and personnel?*
  - Are there resources for ongoing data collection, reporting, and community engagement?*
  
- 6) Who is accountable and how?**
  - How will the impacts be documented and evaluated? (How much did we do? How well did we do it? Is anyone better off? Are we achieving the anticipated outcomes?)*
  - Is continued communication needed? If so, what will that look like?*
  - How could we partner and build relationships with community to ensure sustainable outcomes?*

**NOT ENOUGH TIME? Answer these three essential questions:**

- What are the impacts of this decision?*
- Who will benefit and who will be burdened?*
- Are there strategies to lessen any negative and/or unintended consequences?*

*The HEEAP Sheet is a condensed version of the Equity Toolkit by the Government Alliance on Race & Equity (GARE)  
Red Wing is a participant in the Minnesota GARE cohort of 2016*